

*City of Oakdale Chapter 25 of the Zoning Code*

**APPENDIX B. HELMO STATION-PUD**  
**HELMO STATION PLANNED UNIT DEVELOPMENT**

## Helmo Station Planned Unit Development

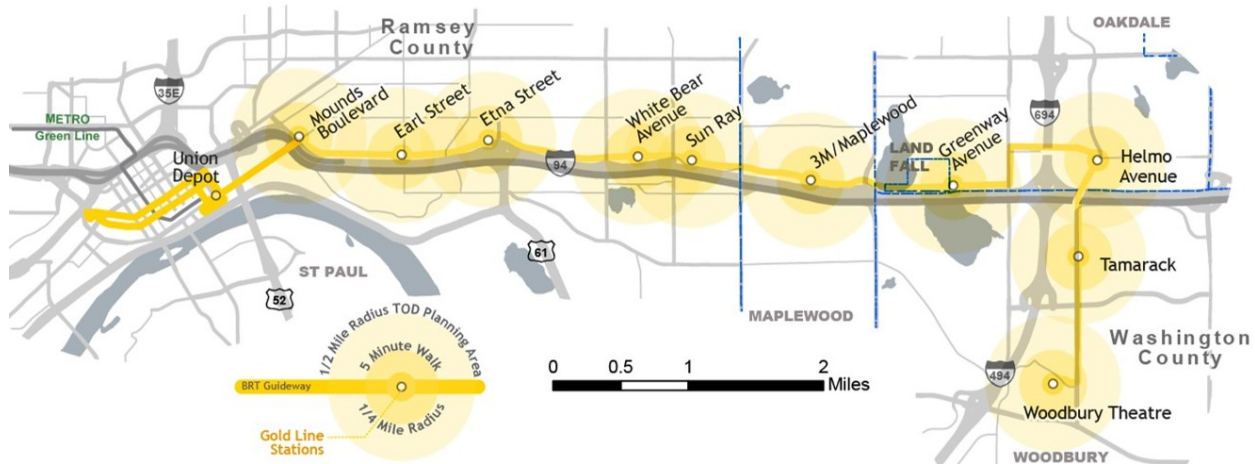
### 1. PURPOSE

- A. The purpose of the Helmo Station Planned Unit Development (PUD) is to:  
Provide the background regarding the planning process that resulted in the Helmo Station Area Plan and PUD; and
2. Specify the regulations for Land Use, Circulation, and Parks and Open Space that shall apply to all property in the Helmo Station-PUD area.
- B. All submittals for platting, subdivision, and site development shall be in substantial conformance, as determined by the City Council, with the regulations in the Helmo Station Planned Unit Development.

### 2. INTRODUCTION

- A. Gold Line Bus Rapid Transit  
The Gold Line Bus Rapid Transit facility is an eleven-mile dedicated guideway that will run from downtown St. Paul to Woodbury (Figure 1). The corridor alignment in Oakdale will follow Hudson Boulevard North, turn onto Hadley Avenue North, follow 4th Street North across Interstate Highway 694, and then turn on Helmo Avenue South to cross a new bridge over Interstate Highway 94 to Bielenberg Drive in Woodbury. There will be two transit stations serving Oakdale: one at Greenway Avenue North and one at Helmo Avenue North.

**FIGURE 1. METRO GOLD LINE BRT SYSTEM AND PLANNING AREA**



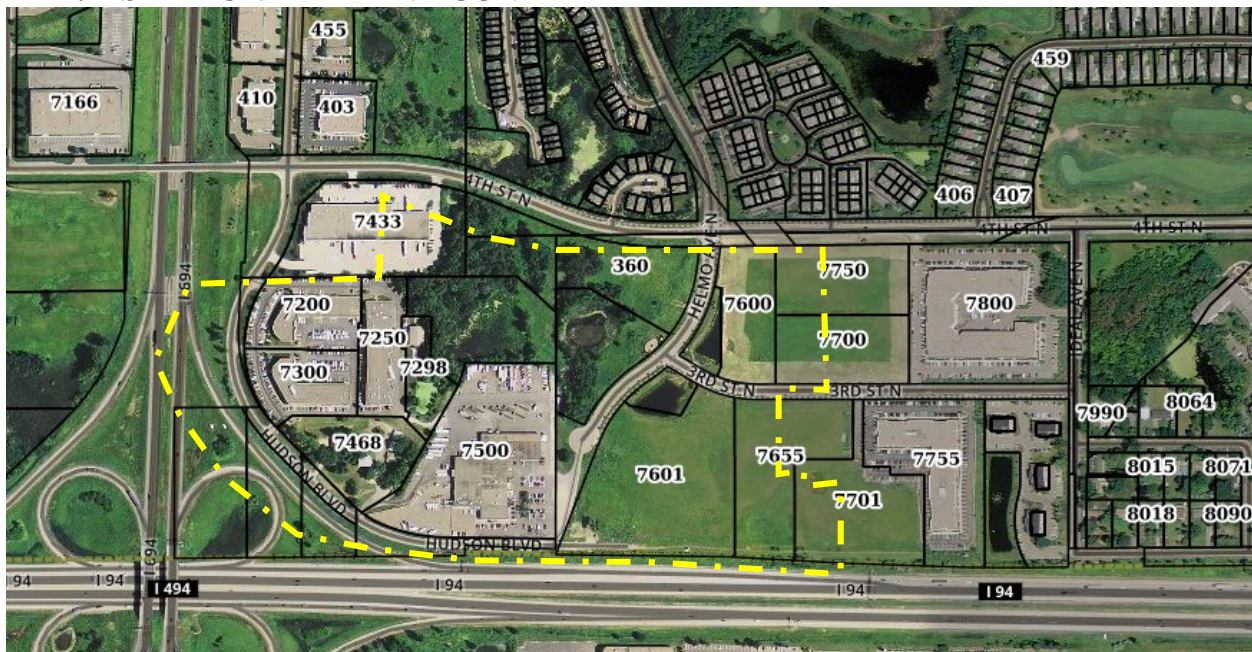
- B. Station Area History and Context  
The Helmo Station area is located north and east of Interstate Highways 94 and 694; south of 4th Street North; and extends to the west edge of The Oaks Business Center (Figure 2). The area is bisected by Helmo Avenue North and 3rd Street North. There are residential townhomes and twin homes north of 4th Street North; an industrial/office park is located to the northwest; and Oak Marsh Golf Course is to the northeast.

Approximately 30 acres on the east and west side of Helmo Avenue North are currently undeveloped. This area was planned and approved for future phases of The Oaks Business Center. The development was approved in 2006 and was to provide a total of approximately 640,000 square feet of office and flex-office space at full build out. By 2018, five buildings on the eastern edge of the campus have been constructed totaling approximately

186,000 square feet. Roadways, stormwater ponding areas, and utility infrastructure have been installed in anticipation of the planned development. The Oaks Business Center was planned in this location to take advantage of access to and visibility from Interstate Highway 94. However, market conditions have not been favorable for the construction of the remaining planned buildings on the campus and the majority of the site remains vacant. Beginning in 2015, the owner of the business park initiated conversations with the City to develop a new plan for the area to potentially include other uses such as multi-family residential buildings, retail, and a hotel. The discussions corresponded with the beginning of the small area planning process that began in 2016.

On the west side of Helmo Avenue North is an existing office/light industrial business park and two legally non-conforming properties – one with residential uses (7468 Hudson Blvd N) and the other with a number of commercial and industrial tenants (7500 Hudson Blvd N).

**FIGURE 2. STATION AREA AND CONTEXT**



#### C. Small Area Planning Process

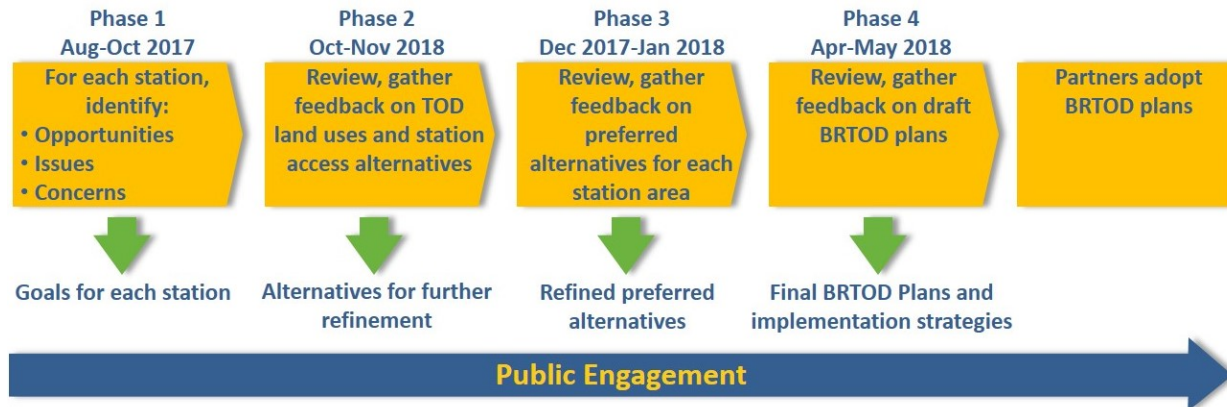
The Gold Line Bus Rapid Transit alignment has created an opportunity to reimagine the land use in this area in a way that will support transit ridership. The Helmo Station Area presents the only large green field development opportunity along the Gold Line as well as significant redevelopment opportunities. Strategic land use planning that leverages the transit infrastructure investment and reflects market conditions will create a unique development for this area of Oakdale.

In 2017 Washington County received a grant to fund station area planning along the eleven-mile Gold Line BRT corridor. In June 2017, the City Council initiated Oakdale's participation in a four-phase station area planning process (Figure 3):

- Phase 1: Identify Opportunities, Issues, and Concerns; Establish Specific Station Area Objectives
- Phase 2: Review Preliminary BRTOD Alternatives; Identify Alternatives for Further Refinement

- Phase 3: Review Preferred Alternative; Identify Preferred Alternative Refinements
  - Phase 4: Review, Finalize, and Adopt BRTOD Plan and Implementation Strategies
- The process included several community open houses, online community engagement, meetings with affected property owners in the subject area, and regular City Council work sessions at each phase of the process.

**FIGURE 3. STATION AREA PLANNING PROCESS**



D. Project Vision and Concept Plan:

The Helmo Station area Concept Plan has been developed to achieve specific goals identified during the planning process. The following goals have been adopted by the City Council for the Helmo Station BRTOD:

1. Establish a multi-modal corridor (for walking, biking, transit & auto)
2. Increase potential ridership (through transit access & new development)
3. Enable station areas to achieve their development potential
4. Identify infrastructure investments and policy changes
5. Maintaining and enhancing open space and trails
6. Preserve existing neighborhoods and quality of life
7. Manage traffic and congestion
8. Create a safe station environment
9. Ensure safe walking and biking
10. Promote compatible development

These goals are embodied in the Helmo Station vision through the following essential elements of Transit Oriented Development:

**Station Hub**—The Station Hub is an area around the station platform with street-oriented retail within or adjacent to high-density multi-family buildings at the intersection of the planned Helmo Avenue bridge. These uses will create an animated 18-hour environment of activity surrounding the station platform. As a result of this activity and eyes-on-the-station, the transit platform will be safer at all times of the day.

**Mixed Use Neighborhood**—The Helmo Station area is a complete neighborhood with multi-family housing surrounding a new neighborhood park and employment (professional and flex office) uses adjacent to the existing Oaks Business Park.

**Connected Parks, Trails, and Open Space**—Open space enhancements and new trail connections create a green setting with recreational amenities for residents and employees in the Station Area and surrounding neighborhoods.

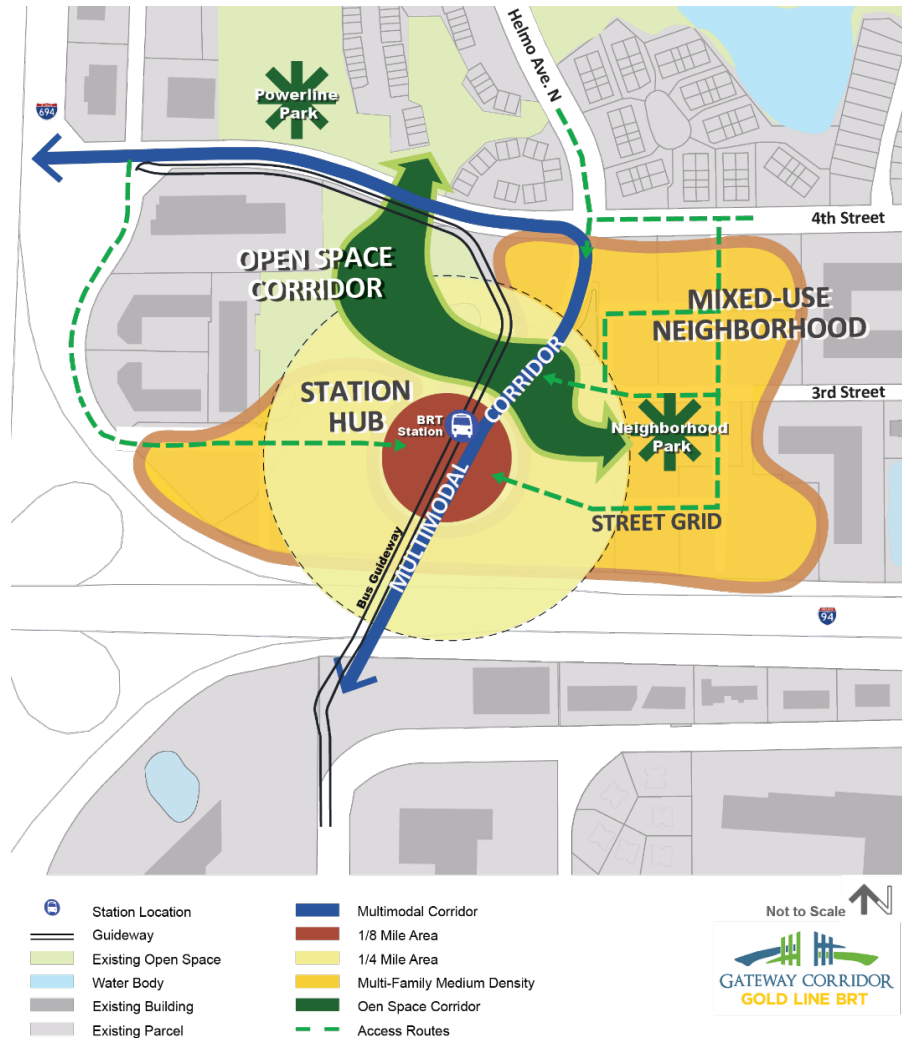
**Street Grid**—Existing streets provide a circulation framework that will be enhanced by a realignment of Hudson Boulevard west of Helmo Avenue and a new street grid east of



Helmo Avenue. These complete streets provide many ways into and out of the neighborhood and improve access to the station, retail, and employment areas.

**Multimodal Corridor**—A multi-use walking and biking trail adjacent to the BRT line links station to station along the entire corridor. A new I-94 bridge crossing will provide improved access for transit, walking, biking, and auto traffic between Oakdale and Woodbury to the south.

**FIGURE 4. STATION AREA VISION DIAGRAM**



**E. Consistency with the Comprehensive Plan**

The City's 2030 Comprehensive Plan was amended on April 24, 2018 guiding the future land use of the project area toward a mixed-use, transit-supportive development that includes multi-family medium and high density residential housing; office-industrial; professional office; and commercial/retail uses along with park and open space amenities to create one cohesive development.

The Helmo Station PUD includes the parcels highlighted on the following map (Figure 5):

**FIGURE 5. HELMO STATION PUD PARCELS**



The Helmo Station BRTOD plan achieves a number of Comprehensive Plan goals:

*Land Use Goal 1: The City shall facilitate the redevelopment and development of certain property.*

*Policy 3. Prepare small area development plans for the following areas to guide public and private investment to achieve a transit oriented development pattern.*

*a. Helmo Avenue North and 4th Street North (Bus Rapid Transit Station Area) Transportation*

*Land Use Goal 3: The City's visual appearance shall incorporate streetscaping and public art.*

*Policy 1. Identify and prioritize areas to enhance streetscaping at major intersections and along key corridors.*

*Policy 2. Develop streetscape design standards for landscaping, lighting, street furniture, sidewalks, and public art in priority areas.*

*Transportation Goal 4: Sidewalks, trails, and bikeways shall be connected within the city and between adjacent cities.*

*Policy 3. Support the construction of new sidewalk and trail connections identified in the Gold Line Station Area Plans.*

*Policy 6. Support the rehabilitation and reconstruction of complete streets that enable safe access for all users, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities.*

*Policy 7. Support the rebuilding of the 4th Street Bridge over I-694 to include space for a dedicated pedestrian walkway and bus rapid transit guideway.*

*Policy 8. Support the addition of a pedestrian walkway adjacent to the 4th Street Bridge over I-694 to improve access to the Helmo transit station from the west side of I-694.*

*Transportation Goal 5: Transit service shall improve mobility options for residents, workers, businesses and transit dependent persons.*

*Policy 1. Collaborate with Metro Transit to assess current transit service and improve transit service for residents, workers, businesses and transit dependent residents.*

*Policy 2. Collaborate with Metro Transit to assess and improve transit facilities and sidewalk and trail connections to and from transit facilities.*

*Policy 3. Collaborate with Metro Transit to develop bus connections between employment and residential areas in the City and the Gold Line bus rapid transit station areas.*

*Parks and Trails Goal 2: Recreational programming, park facilities and open space shall be accessible to all physical abilities and incomes.*

*Policy 1. Develop a plan to ensure programming, parks and open spaces are accessible for all abilities and incomes.*

*Policy 2. Develop a plan to ensure the public use of open space, including wetlands, is open to all pedestrians and bicyclists.*

*Housing Goal 1: All people, regardless of age, income, family status, ability, race or ethnicity, shall have realizable choices and access to a safe, stable, and affordable home.*

*Housing Choice Policies*

*Policy 1. Guide and zone land to facilitate and promote the construction of a full range of housing choices to include single-family detached homes, townhomes, townhomes, duplexes-fourplexes, and multifamily buildings.*

*Policy 2. Promote the development of a variety of housing types within close proximity and safe pedestrian access to shopping and services, including transit, and schools, parks, trails, and open space.*

F. Consistency with Livable Communities Act

The Helmo Station BRTOD project is consistent with the following Livable Communities Act goals established by the Metropolitan Council:

- Interrelating development or redevelopment and transit;
- Interrelating affordable housing and employment growth areas;
- Intensifying land use that leads to more compact development or redevelopment;
- Involving development or redeveloping that mixes incomes of residents in housing, including introducing or reintroducing higher value housing in lower income areas to achieve a mix of housing opportunities; and/or
- Encouraging public infrastructure investments which connect urban neighborhoods and suburban communities, attract private sector development investment in commercial and residential properties adjacent to the public improvement, and provide project area residents with expanded opportunities for private sector employment.

The Helmo Station PUD allows for the development of a new transit oriented mixed use neighborhood centered on the Helmo Station BRT hub and an enhanced open space corridor. The Helmo Station PUD replaces a previous office warehouse PUD, effectively intensifying the land use of the project area into a more compact, efficient development pattern. The new mixed use neighborhood will allow a density of development that supports a range of housing types and price points, as well as opportunities for employment and commerce. The project area infrastructure provides a multimodal corridor supporting the BRT line, walking, biking, and auto traffic. The Helmo Station BRTOD area will connect suburban residents to employment opportunities and additional transit connections in downtown St. Paul via the new Gold Line BRT and connect residents along the Gold Line to employment opportunities in Oakdale.

3. LAND USE

The land use framework diagram (Figure 6) illustrates the new development patterns and identifies the types of station area uses. On many parcels, a mix of vertical uses is permitted. Where parcels contain a vertical mix of uses, the predominant land use is indicated.

#### A. Land Use Goals

1. Maximize development potential based upon existing adjacent uses and site attributes.
2. Maximize utilization of existing and planned improvements such as planned BRT within street rights-of-way, stormwater, and other utilities.

**FIGURE 6. LAND USE FRAMEWORK**



#### B. Residential Land Use Areas

The Helmo Station area offers the opportunity for a significant amount of multi-family development with direct access to the BRT station. Multi-family housing types, include



apartments and townhomes, providing development flexibility and arranged with the highest density nearest the station (generally within 1/8 mile) and lower density adjacent to existing neighborhoods (generally within 1/4 mile of the BRT station).

1. Development shall avoid an institutional, repetitive, ‘apartment complex’ character and shall have varied architecture.
2. Development shall front the BRT station or the neighborhood park to create an urban street edge that defines and creates a vibrant pedestrian friendly public space. Primary building access/lobbies shall be from the street, green spaces, or pedestrian corridors rather than solely from internal parking lots or structures.
3. Parking shall be located behind, within buildings, or in structures. Design techniques that minimize parked car visual impacts from streets and the disruption of the pedestrian environment shall be utilized.
4. Along Helmo Avenue and the realigned Hudson Boulevard, buildings shall be oriented with windows, doors and lobby entries facing toward the street and BRT station.

**TABLE 1. RESIDENTIAL DEVELOPMENT SUMMARY, PERMITTED USES, AND PARKING STANDARDS**

PERMITTED USES	DENSITY	HEIGHT	DEV. TOTAL	PARKING	PARKING TOTAL
	Dwelling units per acre	Stories (max)	# of Units	Spaces/unit	Approx. Spaces
<b>EAST of BRT GUIDEWAY</b>					
<b>Multi-family High Density (HD): Multifamily Buildings</b>	30-50 du/ac	3 to 6*	484-594 units	1.5/unit (max)	726 to 891
<b>Multi-family Medium Density (MD): Townhomes; 3-8 Unit Multifamily Buildings</b>	15-24 du/ac	2 to 3*	69-119 units	2/unit (max)	138-238
<b>WEST of BRT GUIDEWAY</b>					
<b>Multi-family High Density (HD): Multifamily Buildings</b>	30-50 du/ac	4 to 6	232	1.5/unit (max)	348
* See Sec. 3C.c. (p.10)					

C. Residential Development Performance Standards

a. Setbacks

- 1) **Maximum 10-foot Building Setback**  
A maximum 10-foot ground-floor building setback is required where residential front doors and windows are oriented to the sidewalk, park, or public right-of-way. The limited setback from the sidewalk or public areas allows for landscaping, stoops, patios or other semi-public areas that support a safe and inviting public realm and a degree of separation.
- 2) **Along 4<sup>th</sup> Street North**, buildings shall be set back at least 30 feet from the curb line to allow for a perimeter landscape zone that complements existing development on the north side of the roadway.

- 3) Buildings on the parcel at the southwest corner of 4<sup>th</sup> Street North and Helmo Avenue North shall be oriented to Helmo Avenue North
  - b. Active Edges
    - 1) Primary entrances shall be oriented toward the street. Quasi-public terraces, stoops or porches are encouraged.
    - 2) Windows shall be provided along facades.
  - c. Building Height
    - 1) Multifamily Medium Density buildings (including townhomes) are limited to two-floor (maximum) along the 4th Street frontage (three-floor (max) permitted with Council approval).
    - 2) Multi-family High Density buildings along the 4th Street frontage are limited to three residential stories.
    - 3) Other areas: Multi-family High Density buildings may be up to six stories in height.
  - d. Building Materials and Architectural Standards
    - 1) Exterior surface materials of residential buildings shall be subject to the standards for Office and Commercial buildings in Sec. 25-175(c).
    - 2) All building fronts shall include a minimum of four (4) from the following:
      - i. Architectural detailing, such as cornice, awning, parapet, or columns.
      - ii. A visually pleasing primary front entrance that, in addition to doors, shall be accented a minimum of one hundred fifty (150) square feet around the door entrance for single occupancy buildings and a minimum of three hundred (300) square feet for the front of multi-tenant buildings. Entrances shall be clearly articulated and obvious from the street or sidewalk.
      - iii. Contrasting, yet complementary material colors.
      - iv. A combination of horizontal and vertical design features.
      - v. Irregular building shapes.
      - vi. Horizontal offsets of at least 4 feet in depth.
      - vii. Vertical offsets in the roofline of at least four feet.
      - viii. Fenestration at the first floor level which is recessed horizontally at least 1 foot into the façade.
      - ix. Varying roof lines and roof accents.
      - x. Other similar architectural features in the overall architectural concept.
    - 3) Multi-story buildings shall have the ground floor distinguished from the upper floors by having one or more of the following:
      - i. Awning
      - ii. Trellis
      - iii. Arcade
      - iv. Window lintels
      - v. Intermediate cornice line
      - vi. Brick detailing such as quoins or corbels
- D. Retail Land Use Areas
- A limited amount of retail concentrated at the Helmo Station serves existing area residents, new multi-family and employment uses, with direct, convenient auto access from Helmo Avenue and the future Helmo Avenue/BRT bridge. Ground-floor retail uses are located within single-use and mixed-use buildings oriented to the Helmo Station.
1. Buildings shall front primary streets, such as the realigned Hudson Boulevard and Helmo Avenue, to create an urban street edge that defines a pedestrian friendly public

- space. Primary building access shall be oriented to the street, green spaces, or pedestrian corridors rather than to internal parking lots or structures.
2. Parking shall be located behind or within buildings, or in structures. Design techniques that minimize parked-car visual impacts from streets and the disruption of the pedestrian environment shall be utilized.
  3. Retail shall be pedestrian-oriented. Curbside parking is required along the realigned Hudson Boulevard and portions of Helmo Avenue where ground-floor retail uses occur. This will require careful consideration of 'right-sized' travel lane widths and the exclusion of dedicated right-turn lanes.

**TABLE 2. RETAIL DEVELOPMENT SUMMARY, PERMITTED USES, AND PARKING STANDARDS**

PERMITTED USES*	DENSITY	HEIGHT	DEV. TOTAL	PARKING	PARKING TOTAL
	FAR	Stories (max)	Bldg. (sf)	Spaces/1000 sf	Spaces
<b>EAST of BRT GUIDEWAY</b>					
<b>Retail: Sale of Merchandise and Services</b>	Bldg. Ground floor	---	5,000	2.5/1000 sf (max)	13
<b>WEST of BRT GUIDEWAY</b>					
<b>Retail: Sale of Merchandise and Services</b>	.5 FAR/ Bldg. Ground floor	---	20,000 sf 5,000 sf	2.5/1000 sf (max)	63
* See Sec. 3.E. for complete list of permitted uses					

E. Permitted Uses:

1. Animal hospitals, excluding establishments with outside runs.
2. Antique shops.
3. Art galleries.
4. Bakeries.
5. Banks and financial institutions, excluding drive-in tellers.
6. Barber shops.
7. Beauty parlors.
8. Book and stationary stores.
9. Business machine sales and service shops.
10. Camera and photographic supply stores.
11. Catering establishments.
12. Candy and ice cream stores.
13. Clothes pressing and tailoring shop.
14. Clothing and costume rental.
15. Convenience stores.
16. Day Care Centers.
17. Drug stores.
18. Dry cleaning and laundering business of less than 5,000 square feet of floor area.
19. Electrical and household appliance stores, including radio and television sales and service.
20. Employment agencies.
21. Exercise spas or clubs.

22. Fabric stores.
23. Florist shop.
24. Furniture stores, including upholstering when conducted as an incidental part of the principal use.
25. Garden supply, tool, and seed stores.
26. Grocery stores.
27. Hardware stores.
28. Household furnishings, fixtures, appliances, and accessory stores.
29. Interior decorating stores and shops.
30. Jewelry stores.
31. Launderettes and dry cleaning establishments which provide automatic, self-service facilities.
32. Liquor stores, off sale.
33. Locksmith shops.
34. Musical instrument stores and repair shops.
35. Optical stores.
36. Paint and wallpaper stores.
37. Pet shops.
38. Phonograph record and sheet music stores.
39. Photography studios.
40. Picture framing and picture stores.
41. Repair stores and "fix-it" shops which provide services for the repair of home, garden, yard and personal use appliances.
42. Restaurants, including convenience food types and brew pubs.
43. Sporting and camping goods stores, excluding on-site sales of recreational vehicles and trailers.
44. Tailor shops.
45. Taverns.
46. Toy shops.
47. Travel bureaus and transportation ticket offices.

F Prohibited Uses:

1. Any use which emits an obnoxious odor, fumes, noise, or sound which can be heard or smelled outside of any building.
2. Any operation primarily used as a warehouse operation, manufacturing, distilling, refining, smelting, agricultural, industrial, or mining operation; provided however, the foregoing distilling restriction shall not prohibit the brewing of beer or other brewed malt beverages in connection with a brewpub.
3. Pawn shop, precious metal dealer, flea market, salvage store, or auction house.
4. Manufactured home park, trailer court, labor camp, junkyard or stockyard.
5. Mortuary or funeral home.
6. Adult use establishments as defined and regulated in City Code Chapter 25, Article 21.
7. Tattoo parlor.
8. Any unlawful or illegal purpose.
9. Any use that is a public or private nuisance.
10. Second hand stores.
11. Any use that has its primary business an auto service and repair or body shop repair operation.
12. Any fire sale, bankruptcy sale (unless pursuant to a court order) or auction house operation.



13. Automobile and other vehicle sales including used vehicle sales.
  14. Drive-through lanes (serving permitted uses).
  15. Motor fuel sales.
  16. Motor fuel station car washes.
  17. Vending machines.
  18. Game rooms.
  19. Self-storage.
  20. Kennels.
  21. Car washes.
  22. Kiosk sales
- G. Retail Performance Standards
- a. Setbacks
    - 1) Ground floor build-to lines
      - i. Zero-foot Building Setback
        1. Ground-floor building facades must be built directly to the property line and abut the edge of the sidewalk, trail or public use area.
        2. Exceptions to the build-to line criteria are as follows:
          - a. Ground-floor entrances to buildings may be recessed up to five feet behind the build-to line.
          - b. Windows and walls may be recessed up to 18 inches from the build-to line to accommodate columns or other architectural elements that engage the build-to line.
          - c. Interruptions to the build-to line created by passageways to courtyards, parking or other private spaces are permitted.
  - b. Active Edges
 

Active edges are characterized as building frontages with direct entries from the sidewalk and a high degree of transparency. Active edges increase visual and physical interaction between people inside and outside of the buildings, creating a safe and vibrant pedestrian environment. Access to service/loading bays and parking lot/garage entrances are prohibited along designated active edge frontages.

    - i. A minimum of 70 percent transparent glass or screens is required along ground-floor facades, measured from datum line five feet from the ground extending from building edge to building edge; frosted, tinted, reflective glass or other types of glass that diminish transparency are prohibited.
    - ii. Primary entrances to all ground-floor uses shall be oriented to the public right-of-way.
  - c. Building Materials and Architectural Standards
    - 1) Exterior surface materials shall be subject to the standards for Office and Commercial buildings in Sec. 25-175(c).
    - 2) All building fronts shall include a minimum of four (4) from the following:
      - i. Architectural detailing, such as cornice, awning, parapet, or columns.
      - ii. A visually pleasing primary front entrance that, in addition to doors, shall be accented a minimum of one hundred fifty (150) square feet around the door entrance for single occupancy buildings and a minimum of three hundred (300) square feet for the front of multi-tenant buildings. Entrances shall be clearly articulated and obvious from the street or sidewalk.

- iii. Contrasting, yet complementary material colors.
  - iv. A combination of horizontal and vertical design features.
  - v. Irregular building shapes.
  - vi. Horizontal offsets of at least 4 feet in depth.
  - vii. Vertical offsets in the roofline of at least four feet.
  - viii. Fenestration at the first floor level which is recessed horizontally at least 1 foot into the façade.
  - ix. Varying roof lines and roof accents.
  - x. Other similar architectural features in the overall architectural concept.
- 3) Multi-story buildings shall have the ground floor distinguished from the upper floors by having one or more of the following:
- i. Awning
  - ii. Trellis
  - iii. Arcade
  - iv. Window lintels
  - v. Intermediate cornice line
  - vi. Brick detailing such as quoins or corbels

#### H. Employment Land Use Areas

Employment uses consist of traditional professional office (where commercial or professional activities take place but where goods are not produced, sold, or repaired) and flex office (such as office/research and development/light manufacturing), on sites visible from I-94 and adjacent to existing office uses with direct access and visibility from drive-by traffic from 4th Street.

- 1. Buildings shall front primary streets such as 4th Street and the realigned Hudson Boulevard and local streets to create an urban street edge that defines a pedestrian friendly public space. Primary building access/lobbies shall be oriented to the street, green spaces, or pedestrian corridors rather than internal parking lots or structures.
- 2. Parking shall be located behind, within buildings, or in structures. Design techniques that minimize parked car visual impacts from streets and the disruption of the pedestrian environment shall be utilized.
- 3. Parking structures shall be wrapped by office buildings or screened by landscaping or other means.

**TABLE 3. EMPLOYMENT DEVELOPMENT SUMMARY**

<b>PERMITTED USES*</b>	<b>DENSITY</b>	<b>HEIGHT**</b>	<b>DEV. TOTAL</b>	<b>PARKING</b>	<b>PARKING TOTAL</b>
	FAR	Stories (max)	Bldg. (sf)	Spaces/1000 sf	Spaces
<b>EAST of BRT GUIDEWAY</b>					
<b>Flex Parcels Employment: Office/Industrial; Professional Office</b>	0.5 to 1.0 FAR	2 to 6	31,500 sf to 223,844 sf	2/1000 sf (max)	64 to 449
<b>4<sup>th</sup> Street North Employment: Professional Office</b>	0.5 to 1.0 FAR	2	10,000 sf	2/1000 sf (max)	63
<b>WEST of BRT GUIDEWAY</b>					
<b>Employment: Office/Industrial; Professional Office</b>	1.0 FAR	6	94,300 sf	2/1000 sf (max)	189
*See Sec.3.I.					
** See Sec. 3.J.c.					

- I. Permitted Use Definitions
  1. Office/Industrial:
    - a. Light Manufacturing: Any operation which assembles, improves, treats, compounds, and/or packages goods or materials in a manner which does not create a noticeable amount of noise, dust, odor, smoke, glare, or vibration outside of the building in which the activity takes place, which does not require outside storage of goods or materials and which does not generate objectionable amounts of truck traffic.
    - b. Offices: Structures, or portions of structures, in which commercial or professional activities take place but where goods are not produced, sold, or repaired.
    - c. Research and Development and Laboratory Uses: Medical, chemical, electrical, metallurgical or other scientific research conducted in accordance with the provisions of this Ordinance.
  2. Professional Office: Structures, or portions of structures, in which commercial or professional activities take place but where goods are not produced, sold, or repaired.
- J. Employment Performance Standards
  - a. Setbacks
    - 1) Along 4th Street North, buildings shall be set back at least 30-feet from the curb line to allow for a perimeter landscape zone that complements existing development on the north side of the roadway.
    - 2) Where Employment uses are located across the street from Residential uses, buildings shall be set back at least 20 feet from the property line to allow for a landscaped yard.
  - b. Site and Building Design
    - 1) Primary entrances to all ground-floor uses shall be oriented to the public right-of-way.
    - 2) Access to service/loading bays shall not be located on a street frontage across from Residential land uses.
    - 3) Windows shall be provided along facades.
  - c. Building Height

- 1) Buildings are limited to two-floor (maximum) along the 4th Street frontage (three-floor (max) permitted with Council approval).
- d. Building Materials and Architectural Standards
  - 1) Exterior surface materials shall be subject to the standards for Office and Commercial buildings in Sec. 25-175(c).
  - 2) All building fronts shall include a minimum of four (4) from the following:
    - i. Architectural detailing, such as cornice, awning, parapet, or columns.
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    - iii. Contrasting, yet complementary material colors.
    - iv. A combination of horizontal and vertical design features.
    - v. Irregular building shapes.
    - vi. Horizontal offsets of at least 4 feet in depth.
    - vii. Vertical offsets in the roofline of at least four feet.
    - viii. Fenestration at the first floor level which is recessed horizontally at least 1 foot into the façade.
    - ix. Varying roof lines and roof accents.
    - x. Other similar architectural features in the overall architectural concept.
  - 3) Multi-story buildings shall have the ground floor distinguished from the upper floors by having one or more of the following:
    - i. Awning
    - ii. Trellis
    - iii. Arcade
    - iv. Window lintels
    - v. Intermediate cornice line
    - vi. Brick detailing such as quoins or corbels

#### K. Parks and Open Space Land Use Goals

1. Neighborhood Park Goals
  - a. A 1.93-acre park located south of 3rd Street and west of the existing Oaks Business Park stormwater pond is envisioned to include passive areas with a large open lawn and walkway with perimeter landscaping; small plaza/seating area.
  - b. 2.21 acres of existing stormwater ponds (3rd Street and Helmo Avenue) are envisioned as a passive setting with increased tree cover, benches, perimeter landscaping and pathways.
2. Open Space Goals
  - a. The natural area west of the transit station provides an opportunity for nearby nature and passive recreation for the new neighborhood.
  - b. A portion of Battle Creek runs through the area and varied topography may allow for the creation of spaces for intimate wildlife viewing areas as well as broad vistas looking to the north and south.
  - c. This area may be managed to improve the ecological function of the various plant communities while providing trails, boardwalks, and gathering spaces.



L. Station Plaza Land Use Goals

1. The Station Plaza shall consist of a primarily paved area to allow for pass through and flexibility for assembly of staged events/activities and daily use gathering and social interaction.
2. Amenities will be provided such as fixed or moveable seating, tables, and lighting; canopy trees to provide shade and tree cover and possible perimeter plantings or planters to increase visual interest and quality of the public space.
3. Public art may be integrated with the BRT station and shelter design.
4. Consolidated bicycle parking and/or a bike station (covered or enclosed building) with secure bike parking, possibly showers/restrooms, lockers and ancillary uses such as repair services or a café may be provided.

M. BRT Park-and-Ride Land Use Goals

1. Retail and commercial sites with a minimum dimension of 30 feet and oriented to the realigned Hudson Boulevard should be reserved along the north side of the park-and-ride to ensure an active station environment and the continuation of retail and commercial storefronts along the street.
2. Primary park-and-ride access should be located on the rear of the site along the existing Hudson Boulevard and below the planned future Helmo Avenue bridge.

N. Performance Standards for all Uses

1. Screening of Utilities
  - a. All mechanical equipment located on the roof or around the perimeter shall be screened from ground level view with materials that are comparable and compatible with that of the exterior building materials. Mechanical equipment located on the roof shall be screened at a distance of 2.5 times the height of the building.
    - 1) A raised parapet or other architectural feature that is an integral part of the building is encouraged as a method of screening for rooftop mechanical equipment or to soften the rooftop view.
    - 2) Screening for rooftop mechanical equipment shall incorporate similar architectural features of the building and/or be constructed of a material and color compatible with other elements of the building.
  - b. All ground mechanical equipment shall be one hundred percent (100%) screened from view by opaque landscaping or a screen wall shall be provided to be compatible with the architectural treatment of the principle building.
2. Trash Handling
  - a) All trash, recycling and related handling equipment shall be stored within the principal structure, within an attached structure accessible from within the principal structure, or within an unattached structure. Such attached storage area shall be separated from the principal structure by a firewall. Recycling areas shall also be provided. Trash, recycling, and rubbish receptacles shall be totally screened from eye level view from public streets and adjacent residential properties. Such structure shall be of the same material and architecturally harmonious with principal structure and shall be enclosed by a roof and readily served through a door or gate system properly designed and constructed for abusive use.
3. Loading Areas

- a. The perimeter views of all external loading and service areas and any areas of outdoor storage must be screened from residential uses and adjacent public streets and the public front and office sides of all commercial and industrial uses, except at access points. Such screening can be accomplished through:
  - 1) The placement of the building on the lot or the placement of a building on an adjacent lot.
  - 2) Through the use of berming and landscaping (80% opaque at the time of maturity). Planting screens shall consist of healthy, hardy plant materials at least 6 feet in height.
  - 3) If screen walls are proposed, the materials used shall be of similar type, quality, and appearance as that of the principal structure. Such screens shall be at least 6 feet in height and provide a minimum opaqueness of 80 percent.
  - 4) Screen walls that are in disrepair shall be repaired. Planting screens shall be maintained in a neat and healthful condition. Plantings that have died shall be promptly replaced.
4. Site Amenities
  - a. All development shall incorporate at least three of the following:
    - 1) Patterned materials on walkways (on-site)
    - 2) Bicycle racks
    - 3) Trash receptacles (decorative)
    - 4) Pedestrian lighting
    - 5) Fountains, sculptures, mobiles, kiosks, or banners
    - 6) Flower boxes, or container landscaping
5. Parking Lots and Circulation
  - a. All development shall conform to the standards in Chapter 25, Article 18 Sec. 25-161(b).
  - b. Within private development, walkways shall be provided to separate pedestrians and vehicles, and shall link ground level uses within the site to the main building entry point, parking lot, and public sidewalks.
  - c. Where pedestrian walks cross drive aisles, they shall be clearly marked with signage, special paving, landscaping or similar methods.
  - d. All parking areas within the Helmo Station PUD shall be subject to the following standards for landscaping of islands, medians, and parking lot edges:
    - 1) Landscaping shall be distributed throughout the parking lot to define major vehicle and pedestrian routes, provide shade, and break-up large paved areas.
    - 2) A minimum of 1 deciduous shade tree shall be provided for each parking island.
    - 3) A landscaped area to include a mix of deciduous shade trees and understory plantings shall be provided in required parking lot setback areas.
    - 4) Landscaping shall incorporate a variety of deciduous and coniferous trees and shrubs for year-round interest, texture, shape, and seasonal color.
    - 5) Edge treatments should visually screen parked vehicles, but not completely obstruct views into and out of the parking lot. The following landscaping shall be provided:
      - i. At least one row of shade trees spaced evenly at 15 to 20 foot intervals (or appropriate to the selected species) for the length of the parking lots edge. Trees can be clustered.
      - ii. Screening, consisting of continuous planting, alone or in combination with a decorative fence/wall or a landscaped berm.

6. Landscaping
  - a. All development shall conform to the standards for site landscaping in Chapter 25, Article 18 Sec. 25-175 (h) items (1)-(9).
7. Lighting
  - a. All exterior lighting shall be designed and arranged to direct illumination away from adjacent properties. All exterior lighting shall be arranged and designed to illuminate directly below or inboard of the property lines of the property such that the point source of light is not directly discernable by pedestrian or vehicular traffic in the public right of way. Site lighting shall have a maximum height of 30' to the illumination source. Lighting shall be designed such that there is a maximum 0.5 foot – candles at any property line. No offsite glare will be allowed. A photometric plan inclusive of all site lighting and specification sheets for each lighting fixture shall be submitted for review.
8. Signage
  - a. Signage shall conform to the standards in Chapter 25, Article 19: Signs.

#### 4. CIRCULATION

The goal of the circulation framework is to establish the Helmo Station as a hub for transit-oriented development through the creation of complete streets where facilities for all modes—auto, truck, transit, pedestrian, and bicycle—are adequately provided. These complete streets include essential auto and service access and ‘right-sized’ roadway travel lanes to preserve necessary mobility for existing collector and minor reliever roadways, while the new street grid provides access to development parcels on local streets that fosters pedestrian and bicycle friendly mixed-use development. A primary objective of the overall corridor-wide BRT is to integrate walking and biking adjacent to the BRT alignment and connecting BRT stations along the entire corridor.

##### A. Overall Circulation Improvements

1. Include a BRT guideway and multi-use trail on the west side of Helmo Avenue North and on the I-94 bridge crossing to Bielenberg Drive.
2. Realign Hudson Boulevard south of the Crossroads Properties building to provide a direct east/west connection to the Helmo Station/Helmo Avenue bridgehead and to the street grid east of Helmo Avenue.
3. Construct a new street grid east of Helmo Avenue and south of 4th Street providing direct access to development parcels and existing Oaks Business Park.
4. Add or expand trail segments within existing rights-of-way along 4th Street N., 3rd Street, and Helmo Avenue N.

A hierarchy of streets has been established to address both mobility and adjacent land use needs. The circulation diagram (Figure 7) illustrates the street types and locations required to provide station area and development parcel access. It establishes a development context including block scale and massing to support future land uses and a setting for placemaking. These street improvements will contribute to the creation of a distinct and attractive mixed-use transit-supportive district.

**FIGURE 7. CIRCULATION FRAMEWORK DIAGRAM**



## MULTIMODAL CORRIDOR

### B. Multi-Modal Corridor Goal

1. A primary objective of the overall corridor-wide BRTOD Plan is to integrate walking and biking adjacent to the BRT alignment and connecting BRT stations along the entire corridor.

### C. Multi-Modal Corridor Performance Standards

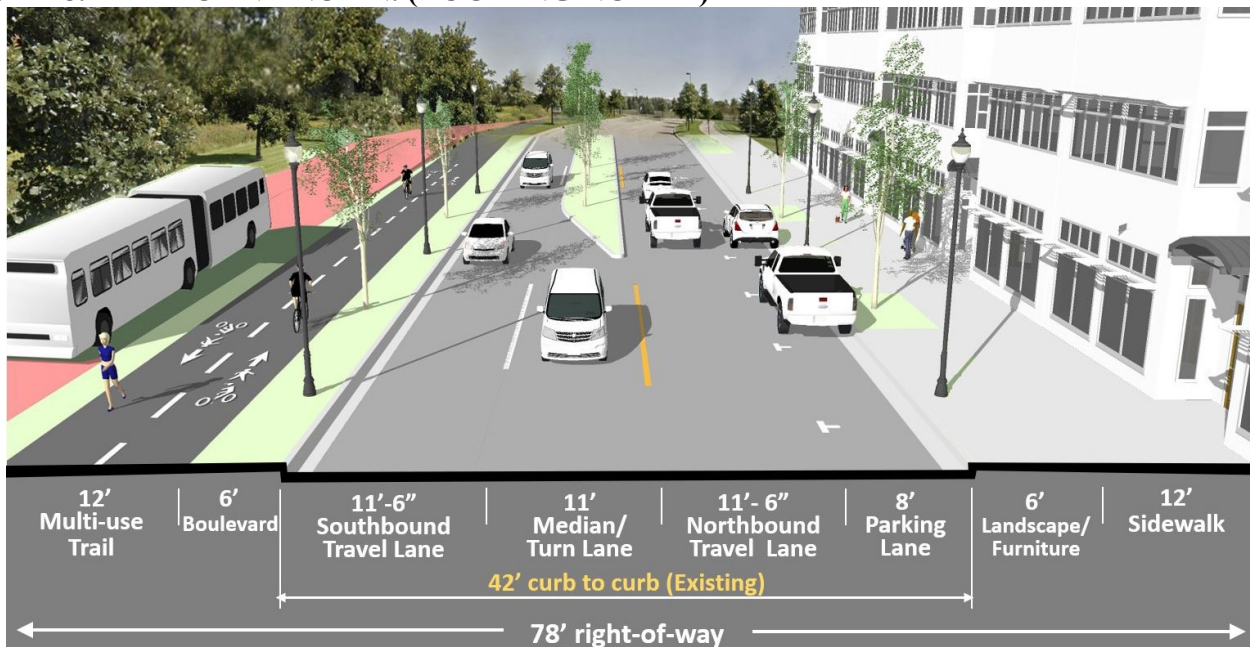
1. Incorporate a multi-use trail into the rights-of-way along Helmo Avenue (north to south from 4th Street to the Helmo Avenue bridge and crossing I-94 to Bielenberg Drive) and 4th Street (east to west from Helmo Avenue to the bridge crossing I-694 to Hadley Avenue N.).



Paralleling the BRT route, this multi-use trail will connect to the station at Greenway Avenue to the east and to the Tamarack Station to the south in Woodbury.

2. Add a 12-foot wide multi-use trail along the west side of Helmo Avenue (widening to 16 feet south into Woodbury) connecting the planned Helmo Avenue bridge multi-use trail to an existing 8-foot multi-use trail on the north side of 4th Street.
3. Expand the existing 8-foot wide trail to a 12-foot wide multi-use trail along the north side of 4th Street from Helmo Avenue to the 4th Street bridge.
4. Helmo Avenue North (Figure 8).
  - a) A 6-foot landscaped boulevard and 12-foot paved asphalt multi-use trail shall be located on the west side of the roadway.
  - b) Pedestrian-scaled street lighting and street trees shall be located between the curb and the trail or sidewalk.
  - c) The existing three-lane roadway shall be re-striped to include a curbside parking lane on the east side of the roadway. Existing curb-to-curb dimensions allow for addition of the parking lane while maintaining adequate travel lanes.
  - d) A wide sidewalk with landscaping and outdoor seating area shall be located adjacent to the curb and parking on the east side of the roadway, to support retail/commercial storefronts.

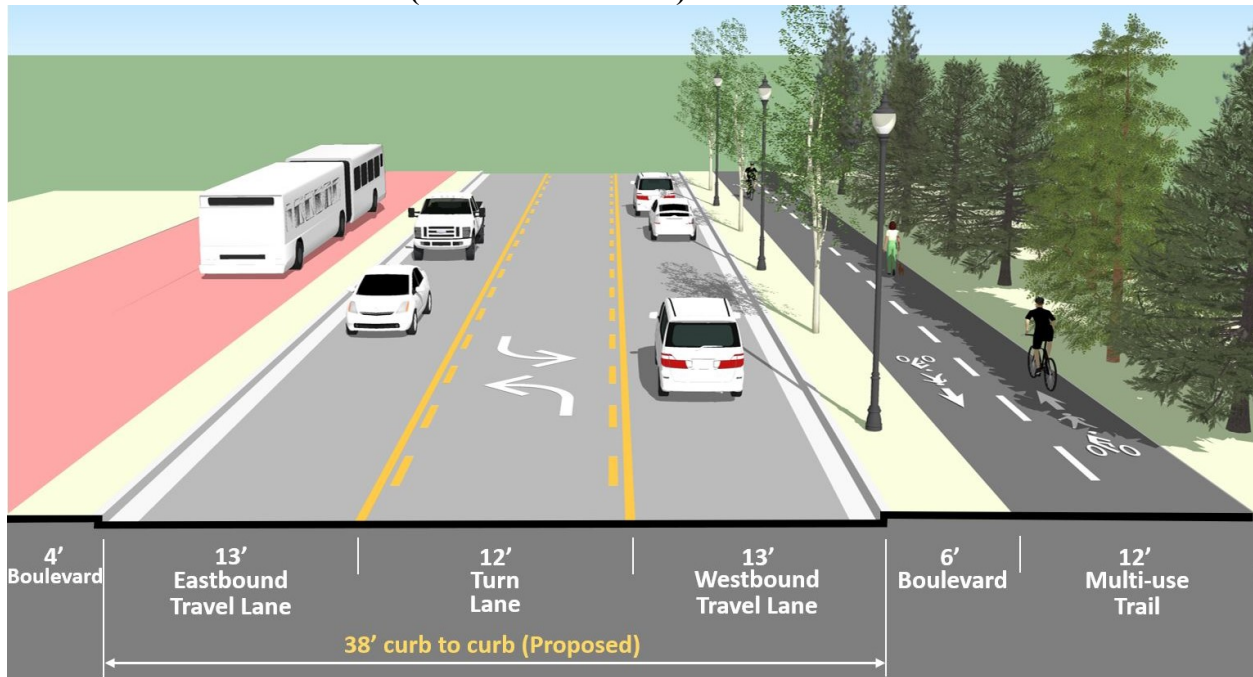
**FIGURE 8. HELMO AVENUE N. (LOOKING NORTH)**



5. 4<sup>th</sup> Street North (Figure 9)
  - a. The roadway may be widened to include an additional turn lane. Existing east/west travel lanes to remain.
  - b. A landscaped boulevard and paved asphalt multi-use trail shall be located on the north side of the roadway from Helmo Avenue to the 4th Street bridge. The existing trail shall be widened by 4 feet and extended an additional 1,250 feet.
  - c. Pedestrian-scaled street lighting and trees shall be located between the curb and the trail on the north side of the roadway.

- d. A landscaped boulevard that provides screening of the guideway with continuous plant material (up to three feet in height) shall be located on the south side of 4<sup>th</sup> Street North between the roadway and the BRT guideway.
- e. A 30-foot landscape setback is required between the edge of the curb and building frontages along 4<sup>th</sup> Street to maintain and enhance the character of the landscaped roadway and compatibility with existing development. A layering of dense evergreen and deciduous plantings is required in the setback and shall be of similar variety and characteristics to the existing roadway plantings.

**FIGURE 9. 4<sup>TH</sup> STREET NORTH (LOOKING WEST)**



## PRIMARY ACCESS ROUTES

### D. Primary Access Route Goals

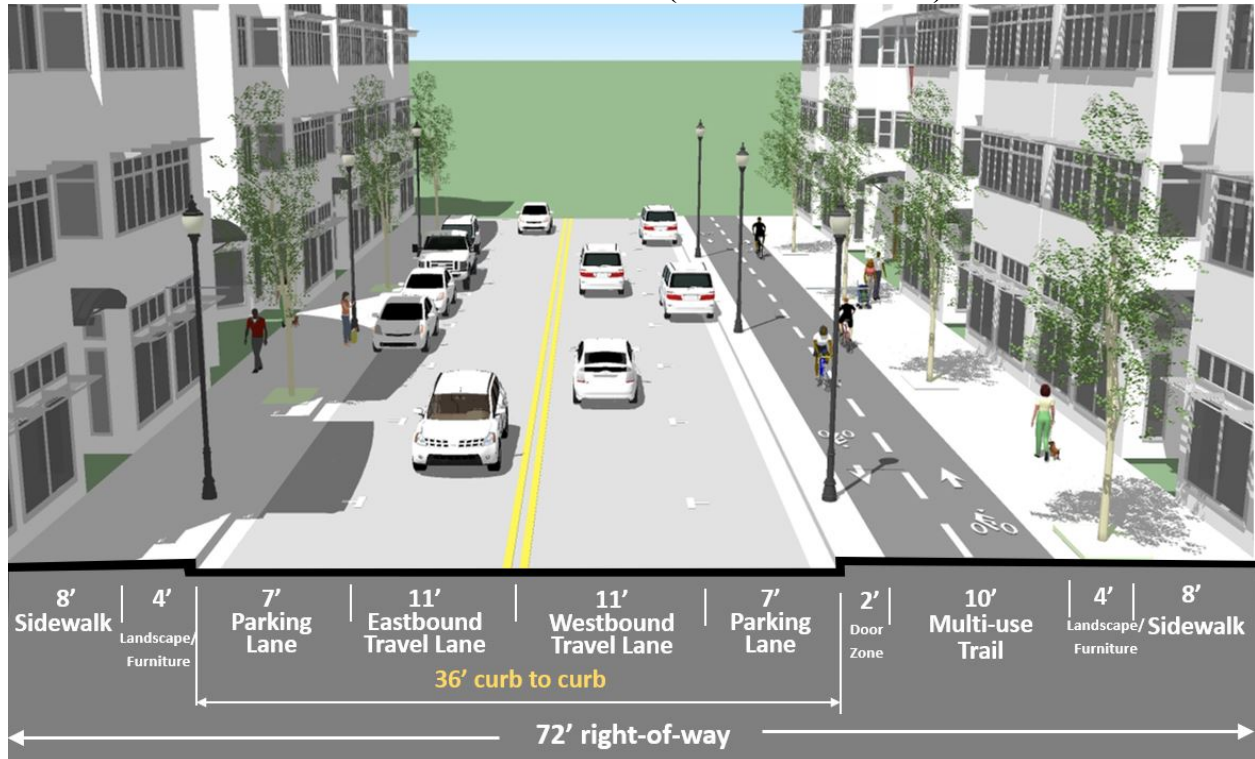
1. Serve as a primary pedestrian and bicycle access route to the station, linking the concentration of existing employment uses within a five-minute walk of the station with those further north and west of the station along I-694.
2. Establish a destination for storefront commercial uses that support an active BRT station environment.
3. Provide commuter access to park-and-ride.

### E. Primary Access Route Performance Standards

1. Hudson Boulevard Extension (Figure 10)
  - a. The Hudson Boulevard Extension shall be a two-way roadway with curbside parking on both sides of the street.
  - b. A multi-use trail shall be located along the north side of the street between curbside parking and a sidewalk with street trees and seating furniture zone.
  - c. Pedestrian-scaled street lighting shall be located between the curb and the multi-use trail.
  - d. A wide sidewalk with landscaping and outdoor seating area shall be located adjacent to the curb and parking on the south side of the street.

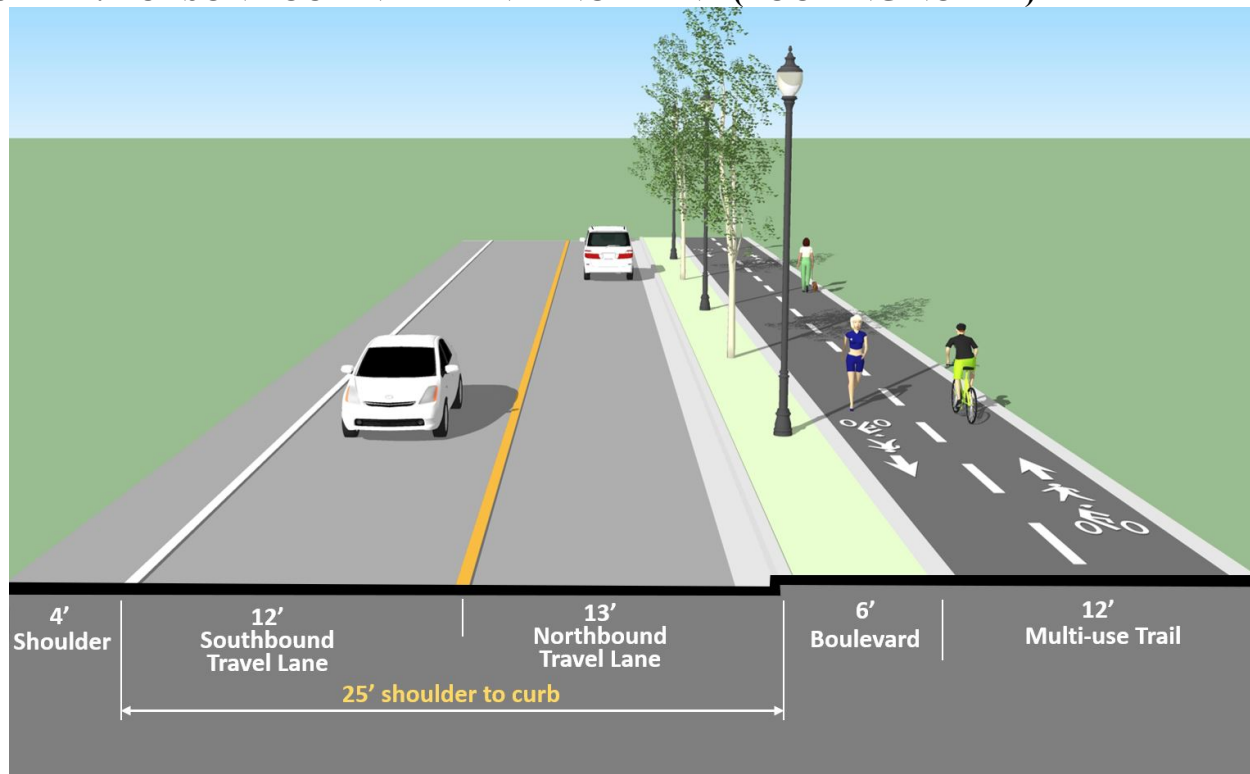
- e. Additional design elements may be incorporated to support retail/commercial storefronts.

**FIGURE 10. HUDSON BOULEVARD EXTENSION (LOOKING WEST)**



2. Hudson Boulevard Enhancement (Figure 11)
  - a. Existing conditions on the west side roadway shoulder and existing travel lanes to remain.
  - b. A new curb and 6-foot landscaped boulevard shall be located on the east side of the roadway with pedestrian-scaled street lighting and street trees located between the curb and a multi-use trail.
  - c. A new 12-foot paved asphalt multi-use trail shall be located on the east side of the roadway between the landscaped boulevard and the existing Crossroads of Oakdale property.

**FIGURE 11. HUDSON BOULEVARD ENHANCEMENT (LOOKING NORTH)**



## NEIGHBORHOOD ACCESS ROUTES

### F. Neighborhood Access Route Goals

1. Ensure that the mobility of existing arterial streets is not degraded.
2. Provide alternate routes for automobiles to disperse traffic away from the intersection of 4th Street and Helmo Avenue N.
3. Establish intimately-scaled blocks that support walking and biking to transit, parks/open space, and commercial uses located at the BRT station.
4. Minimize impacts on existing neighborhoods with improved walking and biking access to station area destinations.
5. Provide direct and convenient access to the future BRT station for all transportation modes.

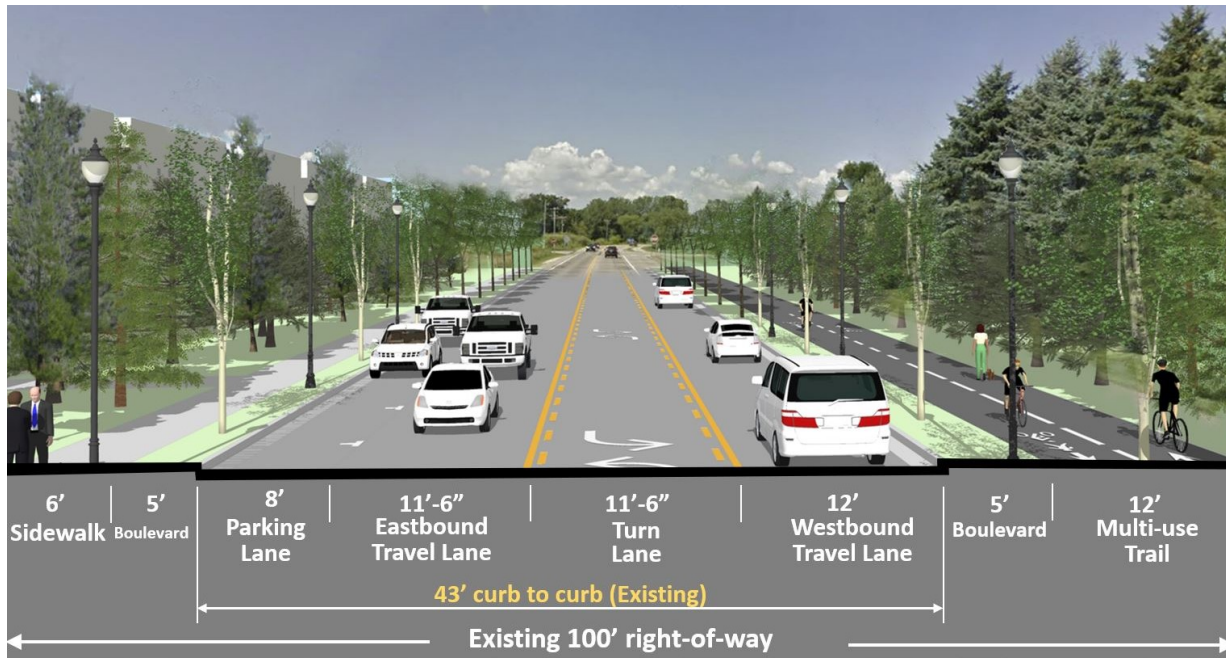
### G. Neighborhood Access Route Performance Standards

1. 4<sup>th</sup> Street North (Figure 12)
  - a. A landscaped boulevard and 4-foot widened asphalt paved multi-use trail shall be located on the north side of the roadway.
  - b. Pedestrian-scaled street lighting and street trees shall be located between the curb and the multi-use trail.
  - c. The existing three-lane roadway may be re-striped to include a curbside parking lane on the south side of the roadway. Existing shoulder-to-curb dimensions allow for addition of the parking lane and adequate travel lanes for autos.
  - d. A sidewalk with landscaped boulevard on the south side of the roadway shall be constructed with future development.



- e. A 30-foot landscape setback is required between the edge of the curb and building frontages along 4<sup>th</sup> Street to maintain and enhance the character of the landscaped roadway and compatibility with existing development. A layering of dense evergreen and deciduous plantings is required in the setback and shall be of similar variety and characteristics to the existing roadway plantings.

**FIGURE 12. 4TH STREET N. (LOOKING WEST)**



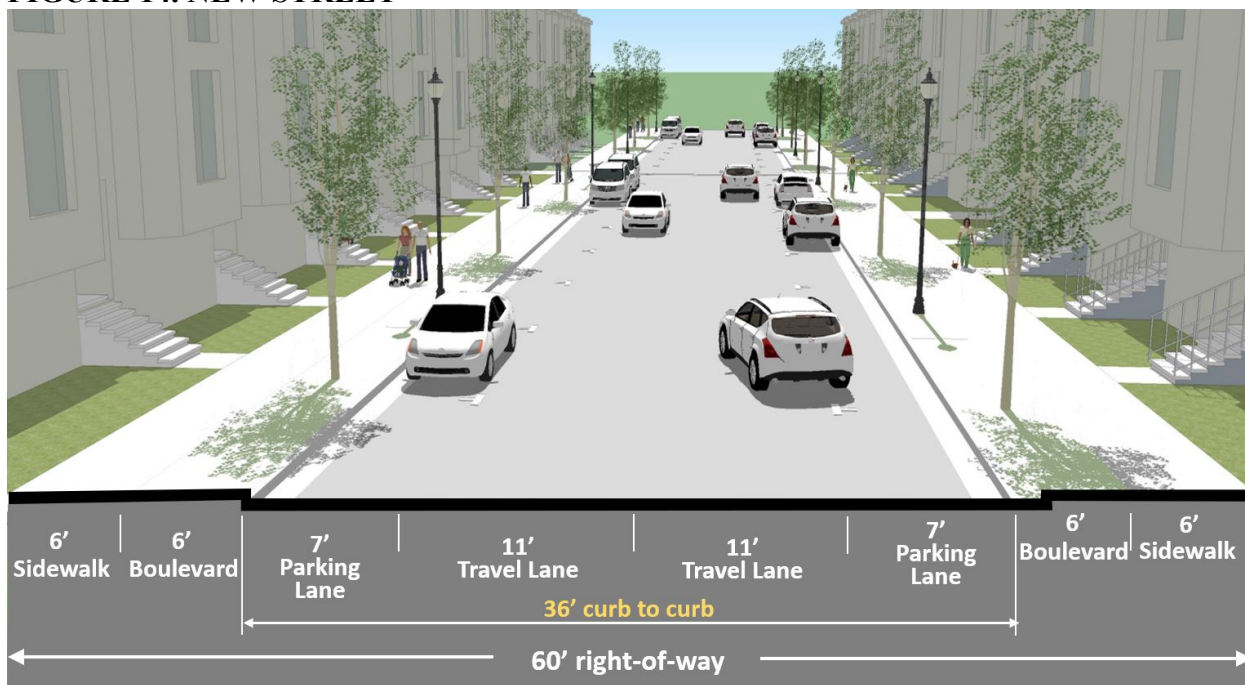
2. 3<sup>rd</sup> Street North (Figure 13)
  - a. The existing travel lanes shall be reduced from 17'-6" to 11'-6" (6-foot reduction for each lane) and an 8-foot parking lane shall be added along the north side of the street.
  - b. The north side curb shall be relocated and replaced, thereby reducing the curb- to-curb distance from 35 feet to 31 feet.
  - c. A new ten-foot multi-use trail shall be located between the boulevard and residential development on the north side of the street.
  - d. Pedestrian-scaled street lighting shall be added on both sides of the street.

**FIGURE 13. 3RD STREET N. (LOOKING WEST)**



3. New Streets 2<sup>nd</sup> Street N., etc. (Figure 14)
  - a. New streets shall consist of a two-lane roadway with curbside parking on both sides of the street.
  - b. Sidewalks and boulevards with street trees shall be located adjacent to curbside parking on both sides of the street.
  - c. Pedestrian-scaled street lighting shall be located between the curb and sidewalk

**FIGURE 14. NEW STREET**



4. Access Ways (Figure 15)
  - a. Access ways shall have a maximum overall width of 40 feet.

- b. Access ways shall consist of a maximum 20-foot paved shared-use pathway for autos, pedestrians and bicycles, and fire vehicle access.
- c. 10-foot wide landscaped plantings beds shall be located between the building and the shared-use pathway; this area may include paved seating areas for outdoor activity and gathering.

**FIGURE 15. ACCESS WAY**

